

INDEX OF STRUCTURES PLANS

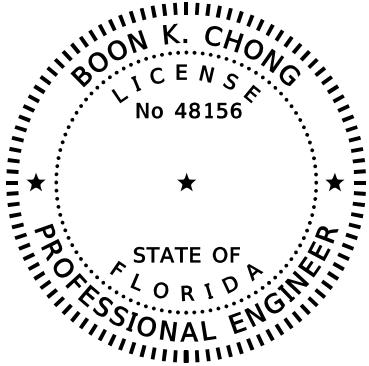
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CITY OF SANIBEL  
COMMUNITY SERVICES DEPARTMENT

CONTRACT PLANS

SANIBEL ISLAND NON-QUALIFIER  
STRUCTURES REPAIR

STRUCTURES PLANS



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GOVERNING STANDARD PLANS:

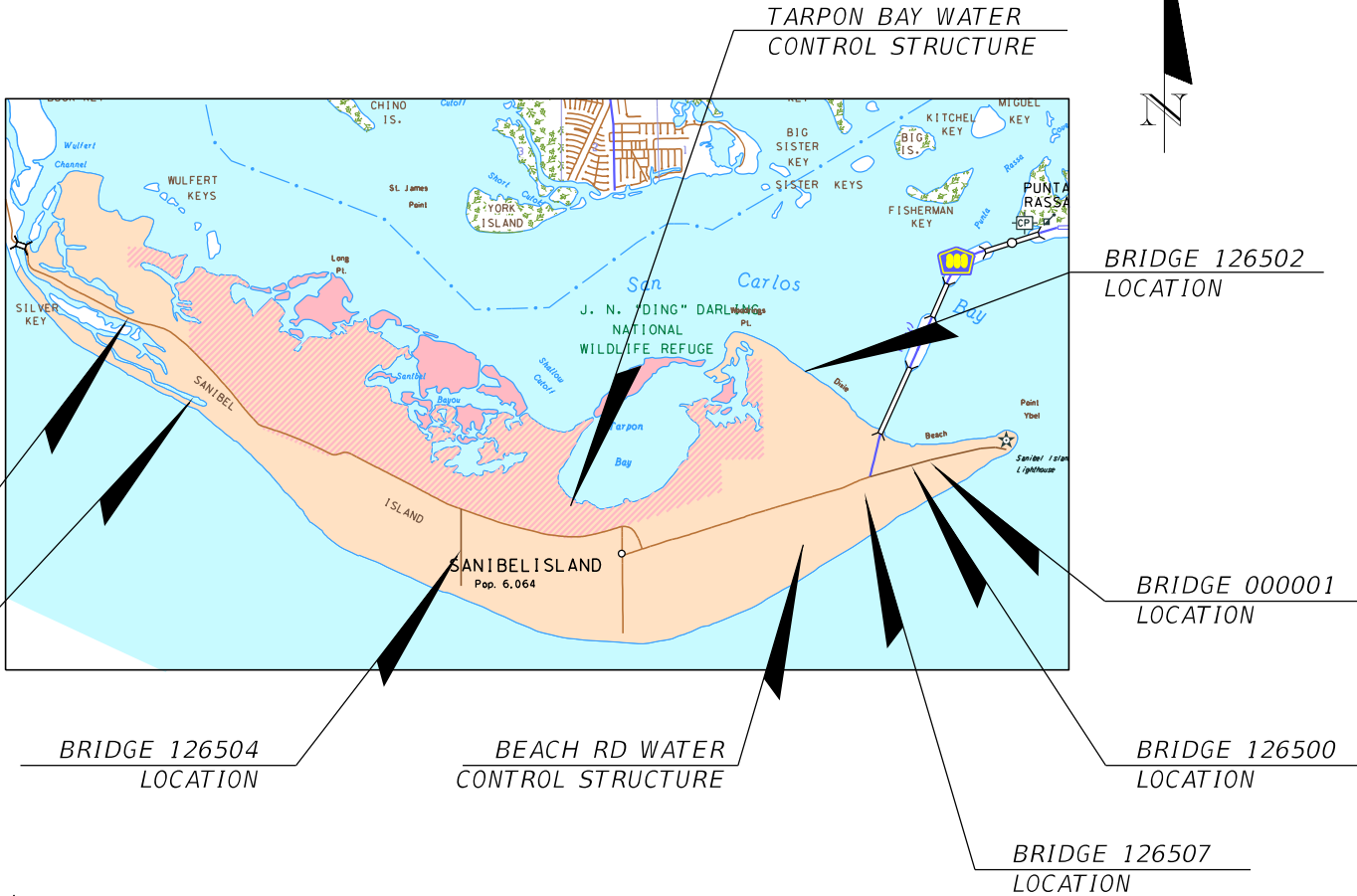
Florida Department of Transportation, FY2024-25 Standard Plans for Road and Bridge Construction and applicable Interim Revisions (IRs).

Standard Plans for Road Construction and associated IRs are available at the following website: <http://www.fdot.gov/design/standardplans>

Standard Plans for Bridge Construction are included in the Structures Plans Component.

GOVERNING STANDARD SPECIFICATIONS:

Florida Department of Transportation, FY2024-25 Standard Specifications for Road and Bridge Construction at the following website: <http://www.fdot.gov/programmanagement/Implemented/SpecBooks>



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STRUCTURES PLANS  
ENGINEER OF RECORD:

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CITY OF SANIBEL PROJECT MANAGER: JOSH HOLLER

FISCAL YEAR	SHEET NO.
25	B-1

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SUMMARY OF LUMP SUM ITEMS					
PAY ITEM NO.	PAY ITEM DESCRIPTION	QUANTITY		DESIGN NOTES	CONSTRUCTION REMARKS
		P	F		
0101-1	MOBILIZATION	1			
102-1	MAINTENANCE OF TRAFFIC	1			

SUMMARY OF STRUCTURE QUANTITIES										
SECTION	PAY ITEM NO.	PAY ITEM DESCRIPTION	LOCATION	UNIT	QUANTITY		TOTAL		DESIGN NOTES	CONSTRUCTION REMARKS
					P	F	P	F		
STRUCTURE 1	401-70-3	RESTORE SPALLED AREAS, LATEX-MODIFIED MORTAR- ACRYLIC		CF	1.9		1.9			
STRUCTURE 8	104-11	FLOATING TURBIDITY BARRIER		LF	40		40			
	457-2-111	CATHODIC PROTECTION INTERGRAL NON-STRUCTURAL PILE JACKET		LF	14		14			
TARPON BAY WEIR	550-10-220	FENCING, TYPE B 5.1 TO 6' STANDARD		LF	430		430			
	550-60-224	FENCE GATE, TYPE B DOUBLE, 18.1 - 20' OPENING		EA	1		1			
	550-60-234	FENCE GATE, TYPE B SLIDING, 18.1 - 20' OPENING		EA	1		1			
BEACH ROAD WEIR	550-10-220	FENCING, TYPE B 5.1 TO 6' STANDARD		LF	126		126			
	550-60-233	FENCE GATE, TYPE B SLIDING, 12.1 - 18' OPENING		EA	1		1			
BRIDGE 126500	120-6	EMBANKMENT		CY	1.0		1.0			
	401-70-3	RESTORE SPALLED AREAS, LATEX-MODIFIED MORTAR- ACRYLIC		CF	1.7		1.7			
	530-3-1	RIPRAP (SAND CEMENT)		CY	1.9		1.9			
BRIDGE 126502	401-70-3	RESTORE SPALLED AREAS, LATEX-MODIFIED MORTAR- ACRYLIC		CF	3.8		3.8			
	530-3-3	RIPRAP (RUBBLE)		TON	18.5		18.5			
BRIDGE 126504	401-70-3	RESTORE SPALLED AREAS, LATEX-MODIFIED MORTAR- ACRYLIC		CF	0.1		0.1			
BRIDGE 126506	401-70-3	RESTORE SPALLED AREAS, LATEX-MODIFIED MORTAR- ACRYLIC		CF	38.8		38.8			
	530-3-3	RIPRAP (RUBBLE)		TON	24.7		24.7			
BRIDGE 126507	401-70-3	RESTORE SPALLED AREAS, LATEX-MODIFIED MORTAR- ACRYLIC		CF	1.5		1.5			
	458-1-21	POUR RUBBER JOINT REHAB		LF	22		22			
	536-1-0	GUARDRAIL TL2		LF	14		14			
	121-70	FLOWABLE FILL		CY	1.0		1.0			

NOTE:

1. Pay Item No. 401-70-3: Payment for concrete spall repairs includes all tools, work, reinforcing steel, etc., and materials including but not limited to, demolition, latex modified mortar, reinforcing steel and mesh incidental to this work. If mechanical couplers are substituted for lap splices, their cost are to be included.

GENERAL NOTES

A. PROJECT LOCATION:

This project consists of culvert and structure repair work on multiple culverts, pedestrian bridge and water control structures on Sanibel Island.

B. SCOPE OF WORK:

- 1. Repair specified culverts and other structures with spall repairs and crack injection.
- 2. Repair specified piles with Cathodic Protection Pile Jackets.
- 3. Remove damaged fence and install new fence.

C. SPECIFICATIONS:

- 1. FDOT Structures Manual, January 2024, and subsequent Structures Design Bulletins
- 2. AASHTO LRFD Bridge Design Specifications, 2020 9th Edition and approved interims.

D. ENVIRONMENT:

Substructure: Extremely aggressive  
Superstructure: Extremely aggressive

E. POLLUTION PREVENTION AND ENVIRONMENTAL CONSIDERATIONS:

- 1. Non-stormwater discharge (including toxic substance and spill reporting): Provide the City with an erosion control plan that will include toxic substance and spill containment, reporting, and response. Specify which management practices and containment methods will be used to prevent potential pollutants (fuel, lubricants, epoxy, sealers, etc.) from spilling into bay waters. If a spill does occur, contact the Engineer immediately. Remove any temporary material used for pollution control during construction at the completion of the project.
- 2. No degradation of water quality is permitted. The Contractor is responsible for any failure to safeguard the applicable water standards. Maintain the applicable water quality standards by preventing the discharge to and/or preventing an increase in the turbidity of the water. Erect and properly maintain appropriate containment devices (i.e. turbidity barriers, silt fences, tarpaulins) to completely enclose the work area during construction.
- 3. Maintain appropriate skimmers and absorbent materials at the work area for immediate deployment in the event of an accidental spill. Properly dispose of waste materials collected in accordance with all local, state, and federal requirements.
- 4. West Indian Manatees may be in the project area. Follow all guidelines in the U.S. Army Corps of Engineers 2011 Standard Manatee Conditions for In-Water Activities to prevent impacts to the manatees.

F. CONSTRUCTION NOTES:

- 1. Perform all pedestrian bridge repairs from waterborne craft. Equipment on the bridge deck will not be permitted for repairs. Construction access is limited unless approved by the Engineer.
- 2. Any proposed staging or storage areas need to be approved by the Engineer. Restore staging areas to original conditions before completion of project at no additional cost to the City.
- 3. Equipment or material shall not be tied to or placed upon any part of the existing bridge or culvert during construction activities without prior approval of the Engineer.
- 4. To avoid damage to the structure during severe weather, properly secure barges and equipment at a location which is away from the bridge and approved by the Engineer.
- 5. All appurtenances and utilities must be protected, remain operational, and intact during construction.
- 6. Provide a cleaning station for concrete pumping trucks.
- 7. Locate all reinforcement in all concrete members prior to beginning repairs.

G. REINFORCING STEEL:

Reinforcing shall be Grade 60 carbon steel per Specifications Section 931.

H. REGULATIONS:

- 1. Prevent debris from falling in the water. Immediately remove debris that falls in the water at the Contractor's own expense.
- 2. Protect and keep operational during construction: all electrical systems, lightning protection systems, signs and utilities.

I. DIMENSIONS:

Dimensions of the structure vary at different elevations and locations. Verify all existing field measurements and conditions shown on the drawings prior to start of construction and ordering of materials. Bring any discrepancy to the Engineer's attention in writing immediately after discovery.

J. TESTING:

All required testing shall be carried out in accordance with the Technical Special Provisions for this project.

K. DISPOSAL OF DEBRIS:

During the progress of the work, remove from the site all discarded activated and non-activated materials, rubbish, concrete, steel, and all other construction debris at the end of each work day. Perform proper classification, packaging, storage, transportation, and disposal of all debris in accordance with all current government regulations and guidelines.

L. IDENTIFICATION OF MEMBERS:

Bents and piers are numbered consecutively from south to north or west to east. Beams are numbered consecutively from left to right when facing the northbound or westbound direction. Beams are identified by the span number followed by the beam number. Piles are identified by the bent number followed by the pile number.

Example: Span 8-2 Beam      Pier 10-1 Column

M. EXISTING PLANS:

- 1. Additional sets of existing plans may be available at the City of Sanibel Island through a public records request to review and copy at the Contractor's expense.

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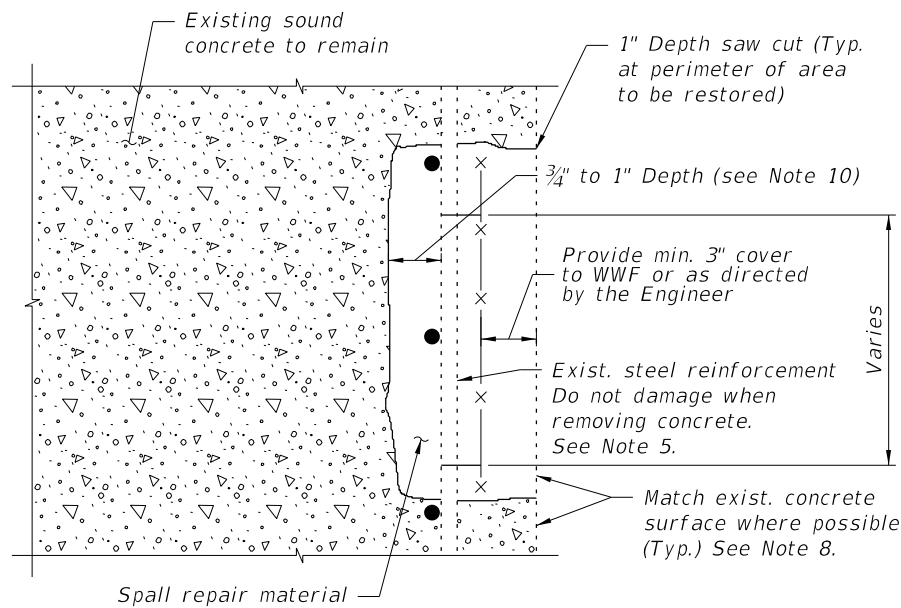
GENERAL NOTES

SANIBEL ISLAND STRUCTURES REPAIR

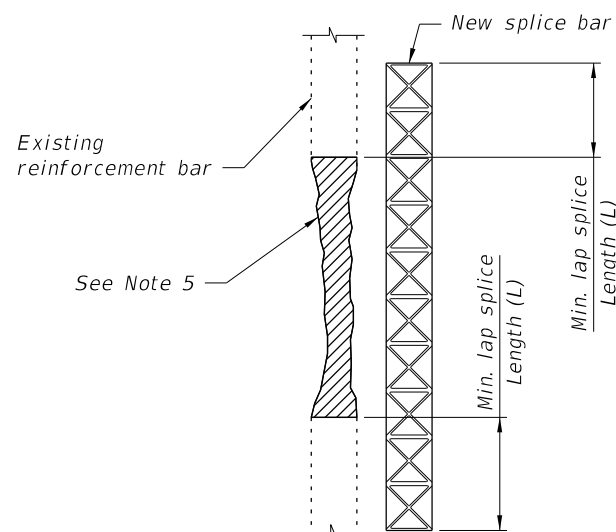
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B-3

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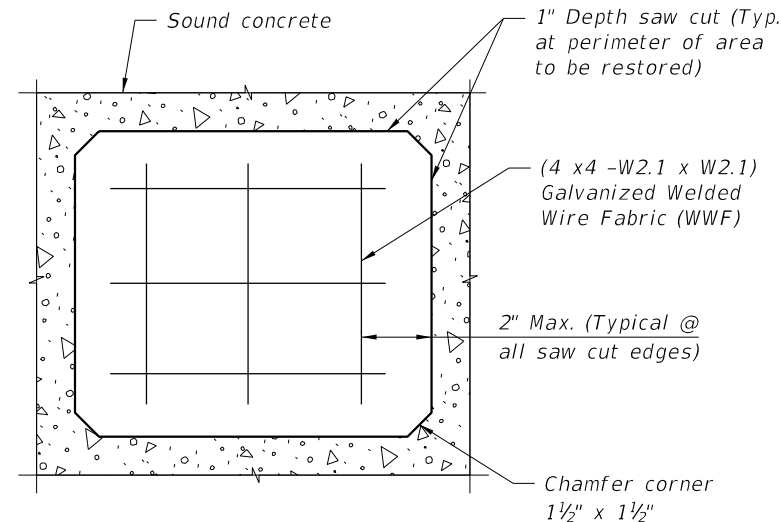


**SPALL REPAIR DETAIL**



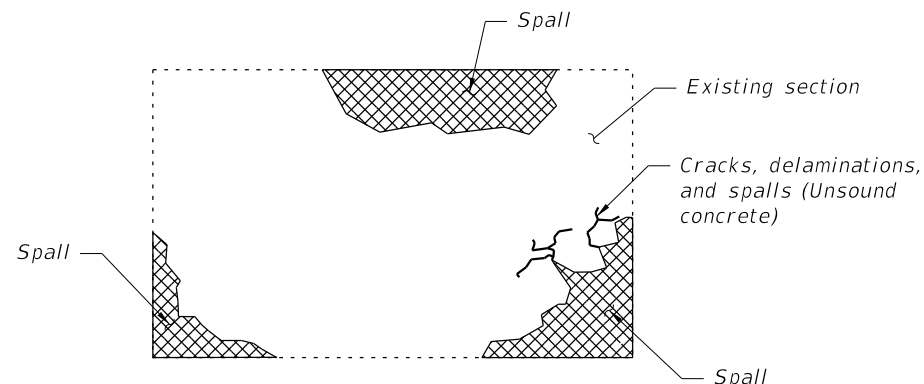
**LAP LENGTH DETAIL**

MIN. SPLICE LENGTHS (L)		
EXISTING BAR SIZE	SPLICE BAR SIZE	SPLICE LENGTH
4	4	16"
5	5	20"
6	6	24"
7	7	28"
8	8	33"
9	9	42"
10	10	53"
11	11	65"

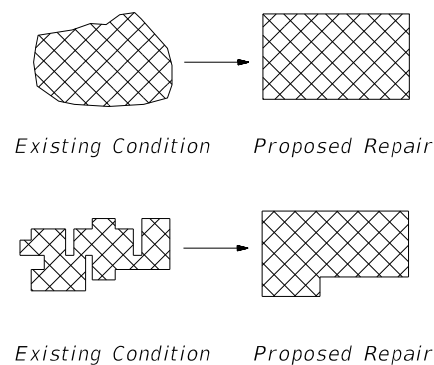


**GALVANIZED WELDED WIRE FABRIC DETAIL**

NOTE: Install welded wire fabric when the depth of concrete removal exceeds 2".

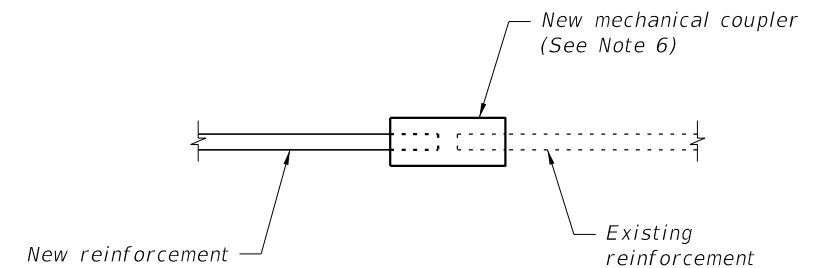


**TYPICAL DELAMINATIONS, CRACKS AND SPALLS**



**TYPICAL DELAMINATIONS, CRACKS AND SPALLS**

NOTE: At edge locations provide right angle cuts.  
No feathered edges. Keep patch configurations as simple as possible.



**MECHANICAL COUPLER DETAIL**

**SPALL REPAIR NOTES:**

- Details are applicable to all orientations: horizontal (including overhead) and vertical.
- Perform spall repair in conformance with TSP T401.
- Only use repair material that is on APL or approved by Engineer and suitable for overhead work.
- For corrosion bleed-out, chip un-sound concrete to the source of bleed-out. At each end of the reinforcing bar chipping operation shall extend beyond the corroded portion of reinforcing exposing 2" to 4" (Max.) of non-corroded reinforcing or as directed by the Engineer.
- When the existing reinforcing steel bar exhibits a 25% or greater reduction in its cross sectional area, use a splice bar in accordance with the Lap Length Detail and Min. Splice Lengths (L).
- As an alternative to splicing the rebars, mechanical couplers may be used for rebars larger than No. 8 bar. Submit proposed coupler to the Engineer for approval prior to use.
- Sound and clearly mark the proposed area for repair to be removed. Obtain Engineer's approval on all areas to be removed prior to beginning work. This includes the proposed depth of removal.
- Apply bonding agent to the surface to be repaired using stiff brushes to work bonding agent into surface. Bonding agent shall contain corrosion inhibitors and be recommended by the manufacturer of the repair material. While bonding agent is tacky, fill prepared areas with Latex Modified Mortar (Acrylic) to original neat lines.
- Complete cleaning operations and repair those areas within the same day without exceeding the bonding agent window of application.
- Where the bond between existing concrete and reinforcement has been destroyed or where more than half the bar circumference is exposed, remove the concrete adjacent to the bar to a depth that will permit the concrete repair mortar to bond to the entire periphery of the bar. Provide to a depth of 1" behind the reinforcement for this purpose.
- Clean steel to near white condition (SSPC SP-10).
- Tie/re-tie all exposed steel (new and old).

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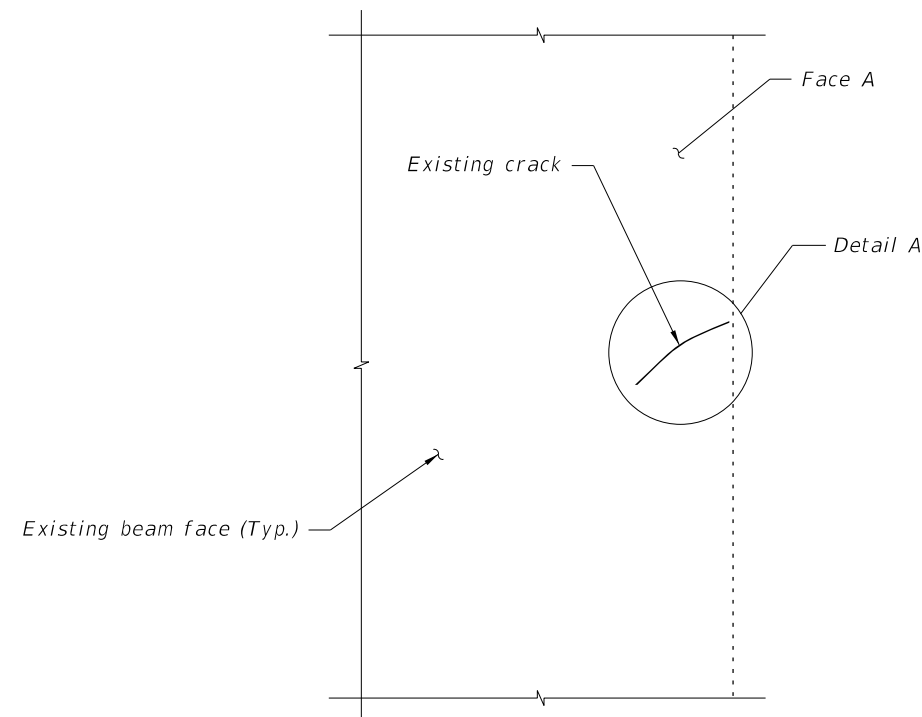
**SPALL REPAIR DETAILS**  
**SANIBEL ISLAND STRUCTURES REPAIR**

SHEET  
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B-4

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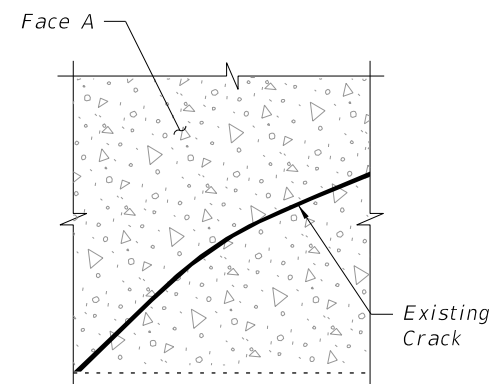




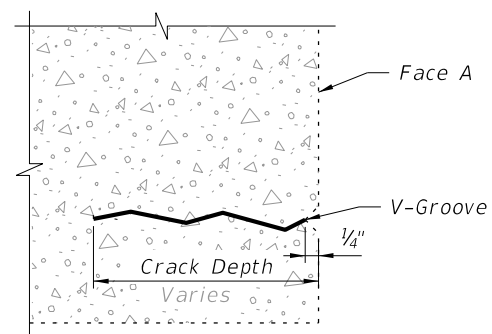
BEAM ELEVATION (Typ.)

NOTES:

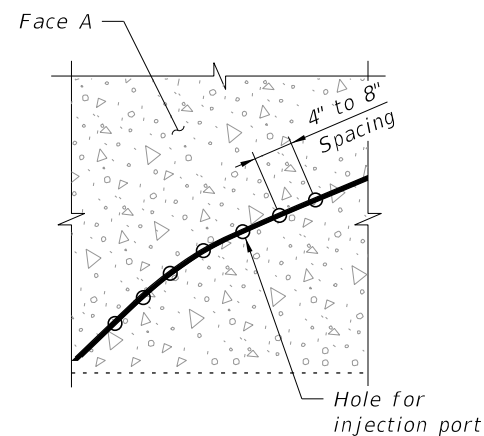
1. "V" Groove the concrete surface along the cracks approximately  $\frac{1}{4}$ " in depth. Clean the V-Groove using air pressure method.
2. Install the injection ports and seal surface of crack with epoxy compound per FDOT Standard Specifications, Section 411. Extend above concrete surface a minimum of  $\frac{1}{16}$ " and extend a minimum of 1" to either side of crack. Allow a minimum of six hours curing before initiating injection process.
3. Inject the epoxy compound per FDOT Standard Specifications, Section 411, and allow minimum of six hours curing time.
4. Perform testing and clean concrete per FDOT Specification 411.
5. See respective bridge plan sheets for crack repair quantities and locations. Submit field survey of actual crack locations with crack length and width for Engineer's review and approval.



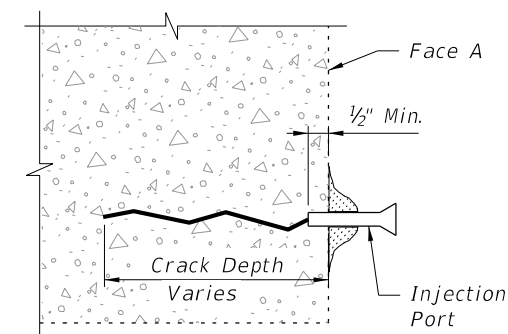
DETAIL A  
(FACE OF BEAM)



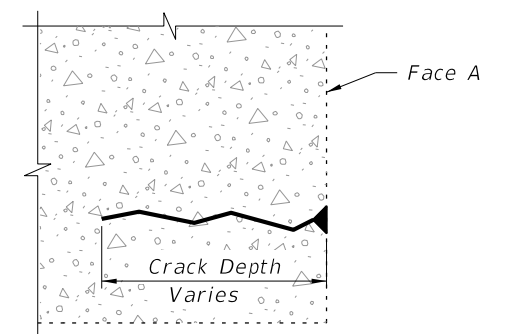
SECTION THROUGH  
CRACK DEPTH



DETAIL A  
(FACE OF BEAM)



SECTION THROUGH  
CRACK DEPTH

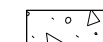


REPAIRED CRACK

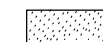
EXISTING CONDITION

PROPOSED REPAIR

LEGEND:



Concrete



Type F-1 Epoxy

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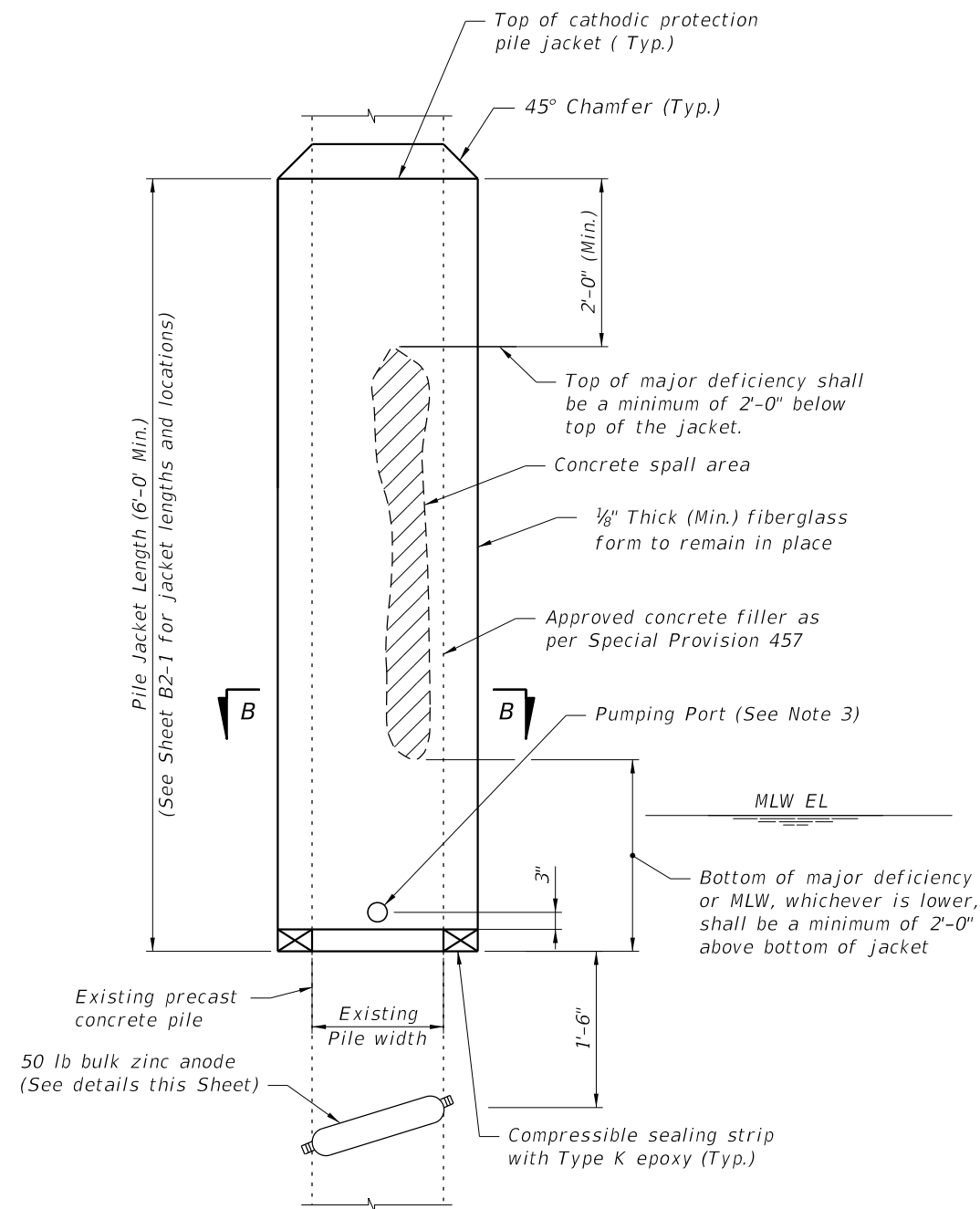
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**CRACK REPAIR DETAILS**  
**SANIBEL ISLAND STRUCTURES REPAIR**

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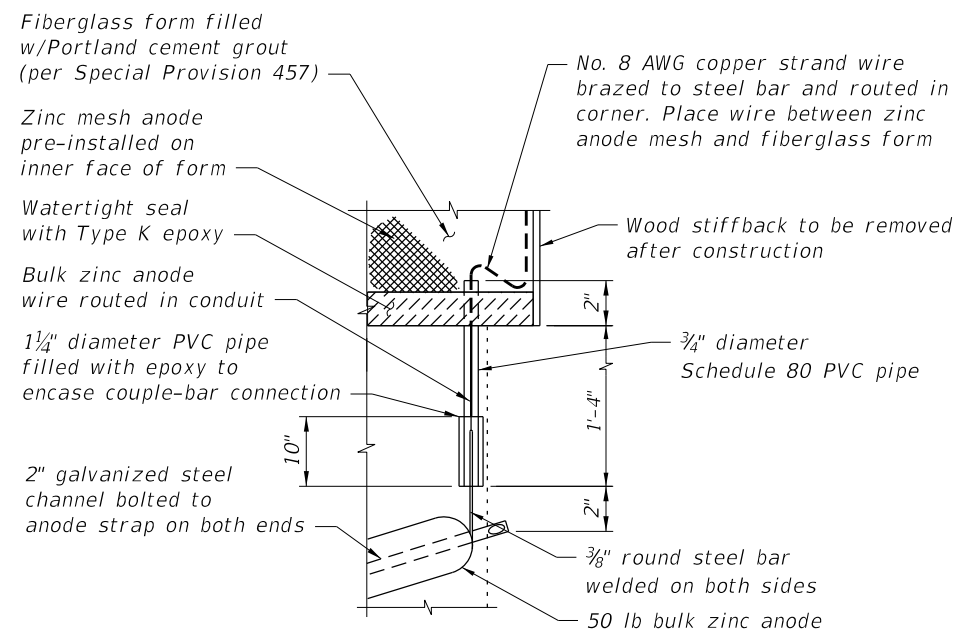
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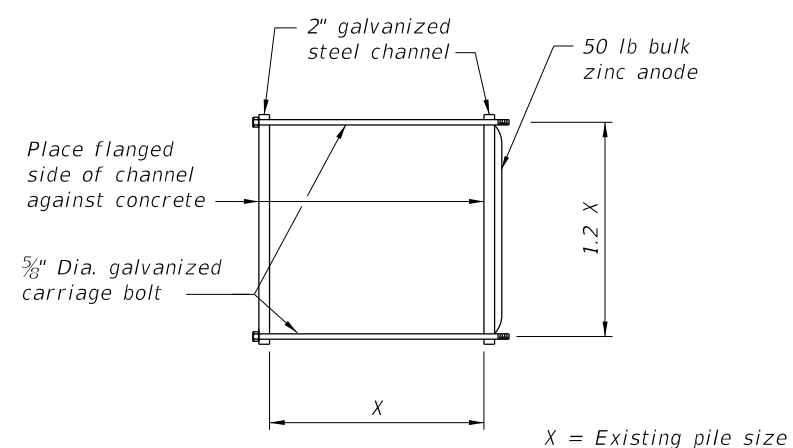


### CATHODIC PROTECTION PILE JACKET

(Cathodic protection details and wiring not shown, see Sheet B-7)



### BULK ZINC ANODE SPLICE DETAIL



### BULK ZINC ANODE ASSEMBLY

#### NOTES:

1. Drill punch holes for bolts in 2" channel prior to hot dip galvanizing.
2. Inside dimensions are shown.
3. Anode must be able to fasten around existing pile.

### PILE JACKET INSTALLATION NOTES:

1. Field verify existing pile dimensions, deficiencies and actual jacket lengths with acceptance of the Engineer prior to ordering materials.
2. Place zinc anode mesh and bulk zinc anode as shown on this sheet and Sheet B-7.
3. Stagger pumping ports on opposite sides of jacket at elevation intervals of no more than four feet.
4. Center the jackets around the piles to a 1" tolerance.
5. Installation of the Galvanic Cathodic Protection System shall be in accordance with the manufacturer's instructions, Section 457 of the FDOT Specifications and TSP T457A.
6. Locate fiberglass form seam on the same face as terminal box.
7. Use only non-metallic fasteners inside the Cathodic Protection Jacket.
8. Provide holes in the wood stiff backs to relieve pressure on the standoff heads, so the form will not be deformed or misaligned.
9. Construct jacket chamfer with a neat line and free of cracks. If cracking occurs, then remove chamfer material and replace at discretion of Engineer.
10. Do not repair more than one pile in one bent simultaneously.
11. Ensure that CP specialist is present at the time of jacket pour to assure shorts do not exist.
12. See Sheet B-7 for Section B-B.

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**CATHODIC PROTECTION  
 PILE JACKET DETAILS (1 OF 2)**  
**SANIBEL ISLAND STRUCTURES REPAIR**

SHEET  
 NO.

B-6

Default



\* See Section B-B. This dimension is to the inside face of the fiberglass jacket. See Sheet B2-1 for jacket lengths.



Wire splice

0.1 Ohm Shunt

Negative connection to existing structural reinforcing steel

Terminal Box SCH 80 PVC (5"x5"x3" Min. PVC Box)

Current Measurement Terminal Bolts (Provide 1/4" Dia. Type 316 SS bolt, nut and washer)

Existing bent cap

1/4" Diameter Type 316 SS bolt, nut and washers

Negative connections to existing and new steel

Existing pile

45° Chamfer

Positive connections to bulk zinc anode

Positive connections to zinc mesh

New cathodic protection pile jacket

DETAIL A

CATHODIC PROTECTION WIRING NOTES:

1. See TSP T457A for installation of current measurement device in terminal box.
2. Use black insulation if it is a negative connection to existing structure reinforcement.

PILE JACKET INSTALLATION NOTES:

1. *Perform the pile jacket survey to confirm the proposed length of each jacket. Conduct the survey above water and underwater with the use of divers. Obtain Engineer's approval prior to ordering of the jackets.*
2. *Mount terminal box above the splash zone on side of the existing concrete pile. Install conduit to terminal box flush on the existing surfaces. Splicing of cables or wire will not be permitted.*
3. *Excavation below the mudline, around the existing pile may be necessary to install pile jackets. This shall be done at no additional cost to the City of Sanibel.*

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CATHODIC PROTECTION  
PILE JACKET DETAILS (2 OF 2)

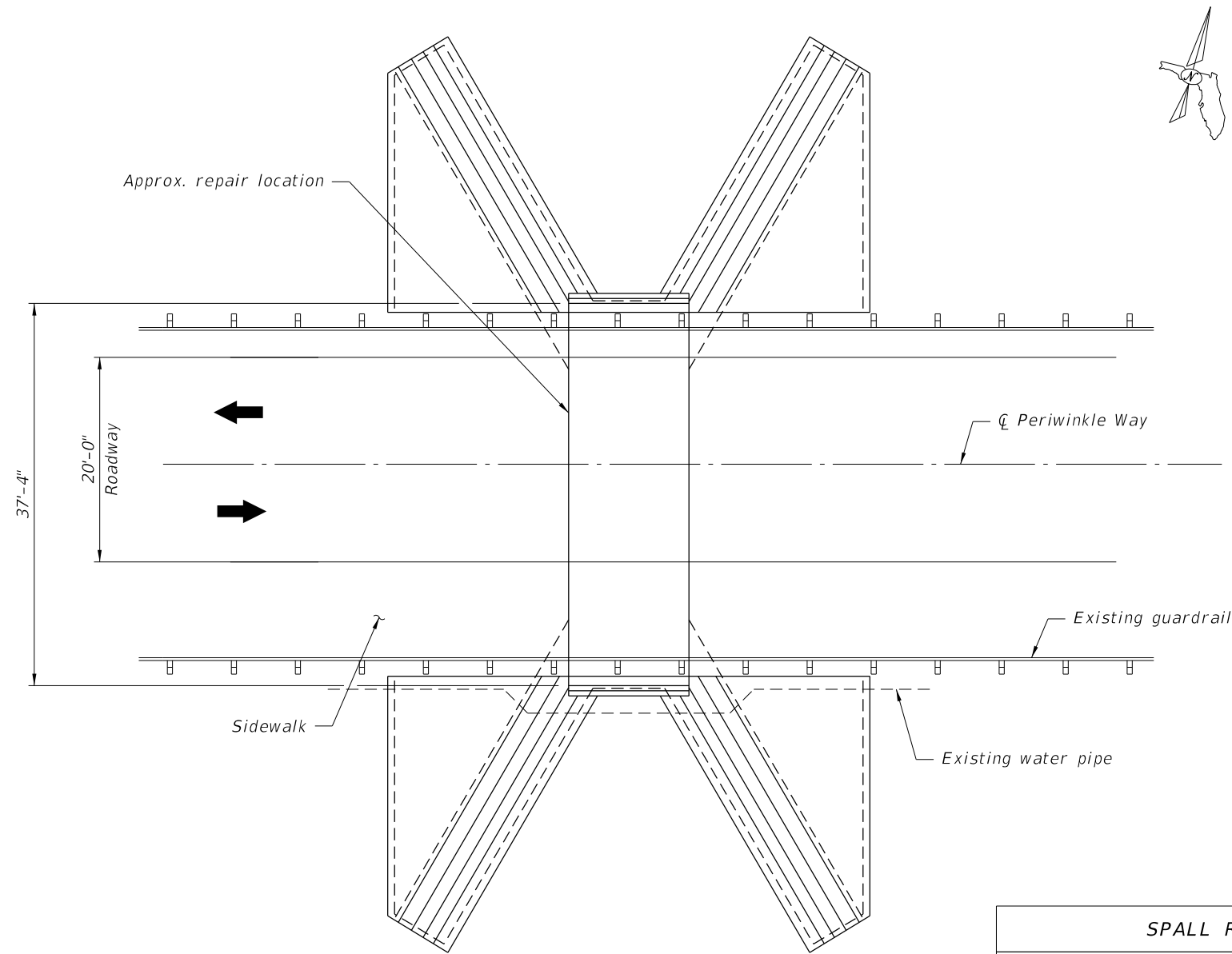
## SANIBEL ISLAND STRUCTURES REPAIR

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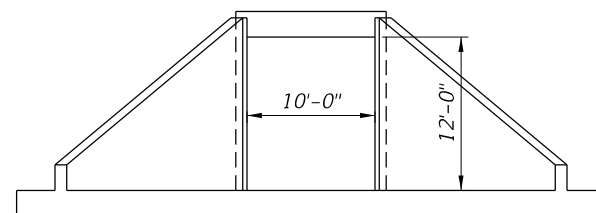
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PLAN

SPALL REPAIR - BRIDGE NO. 000001			
LOCATION	HEIGHT (in.)	LENGTH (in.)	DEPTH (in.)
Culvert Wall 1 near South end	18	36	5



ELEVATION

NOTES:

- See Sheet B-4 for Spall Repair Details.

BRIDGE NO. 000001

NO.	DATE	BY	REVISIONS - DESCRIPTION

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FAX. (239) 332-4798  
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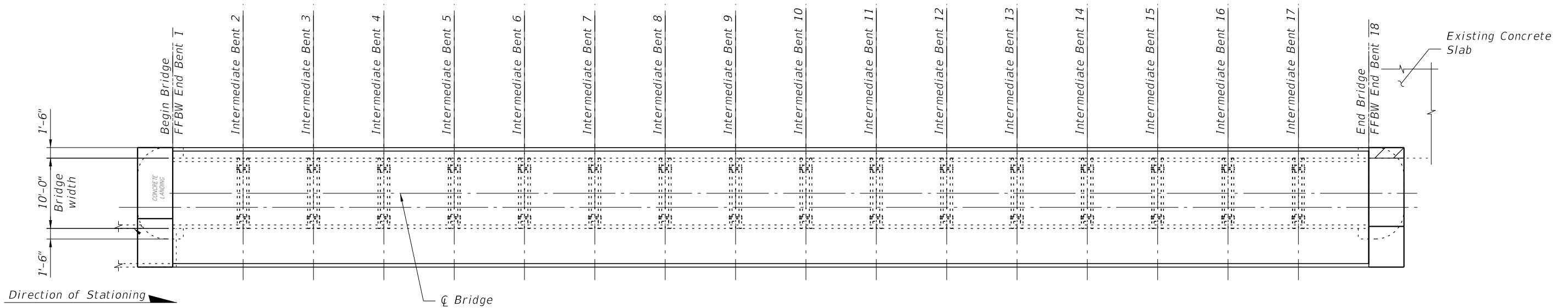
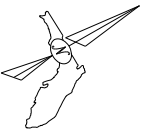
PLAN AND ELEVATION

SANIBEL ISLAND STRUCTURES REPAIR

SHEET NO.

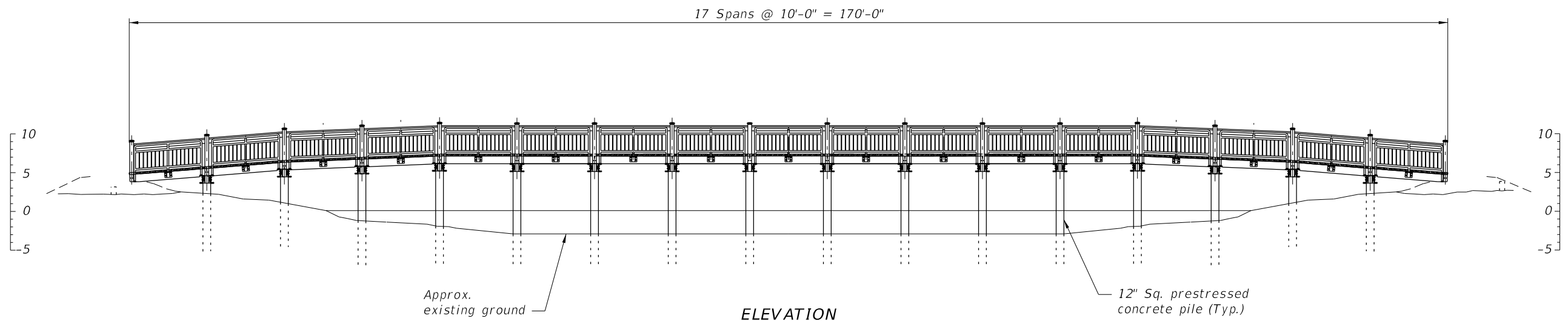
B1-1





PLAN

PILE JACKET LOCATIONS - BRIDGE NO. 000008	
PILE NO.	LENGTH (in.)
14-1	6
8-2	8



ELEVATION

NOTE:  
See Sheets B-6 and B-7 for Cathodic  
Protection Pile Jacket Details.

BRIDGE NO. 000008

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PLAN AND ELEVATION

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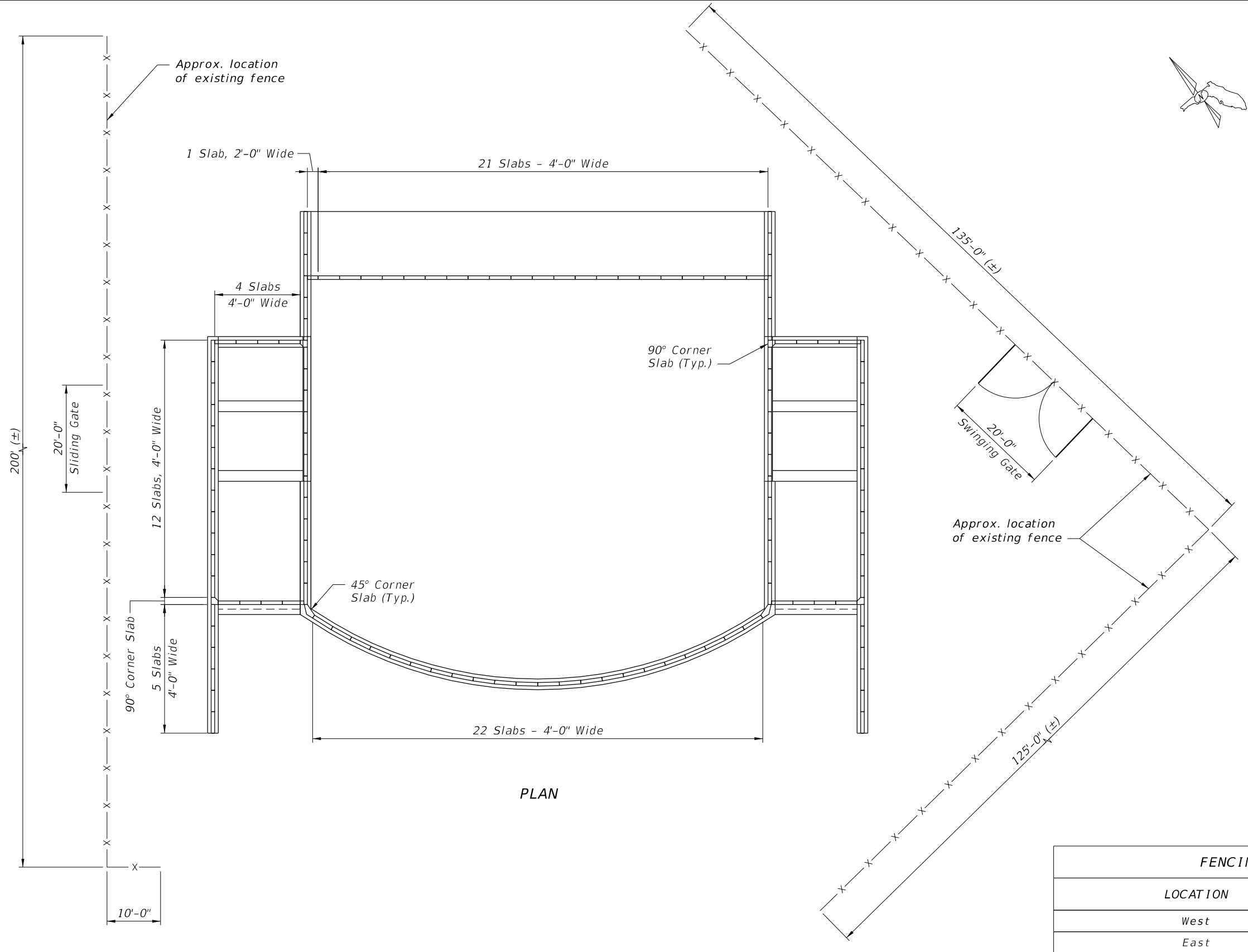
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SHEET  
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**B2-1**

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FENCING	
LOCATION	LENGTH (ft.)
West	190
East	240

TARPON BAY WATER  
CONTROL STRUCTURE

NO.	DATE	BY	REVISIONS - DESCRIPTION

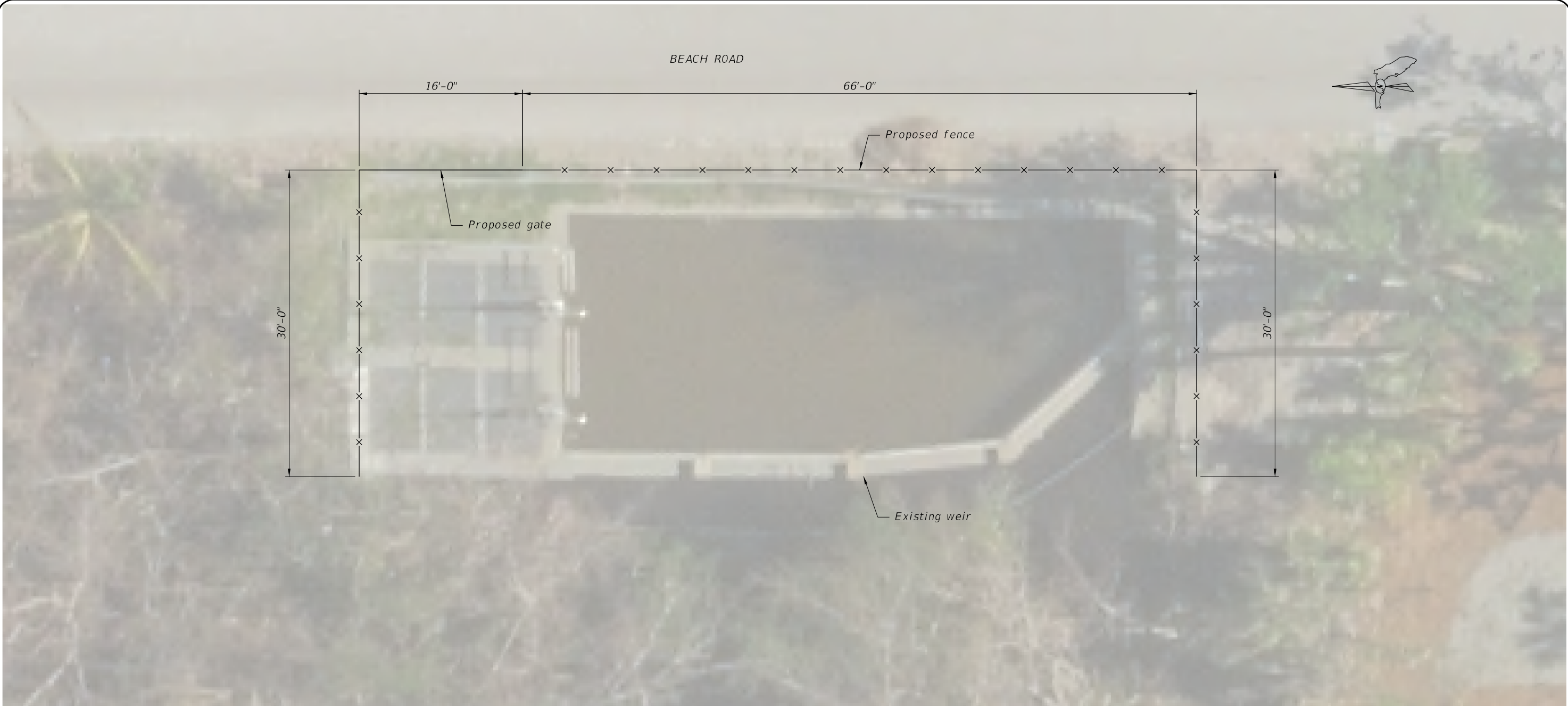
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**TARPON BAY WEIR PLAN**  
**SANIBEL ISLAND STRUCTURES REPAIR**

SHEET  
NO.  
**B3-1**



FENCING	
LOCATION	LENGTH (ft.)
Front Face	126

BEACH RD WATER  
CONTROL STRUCTURE

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BEACH RD WEIR PLAN

SANIBEL ISLAND STRUCTURES REPAIR

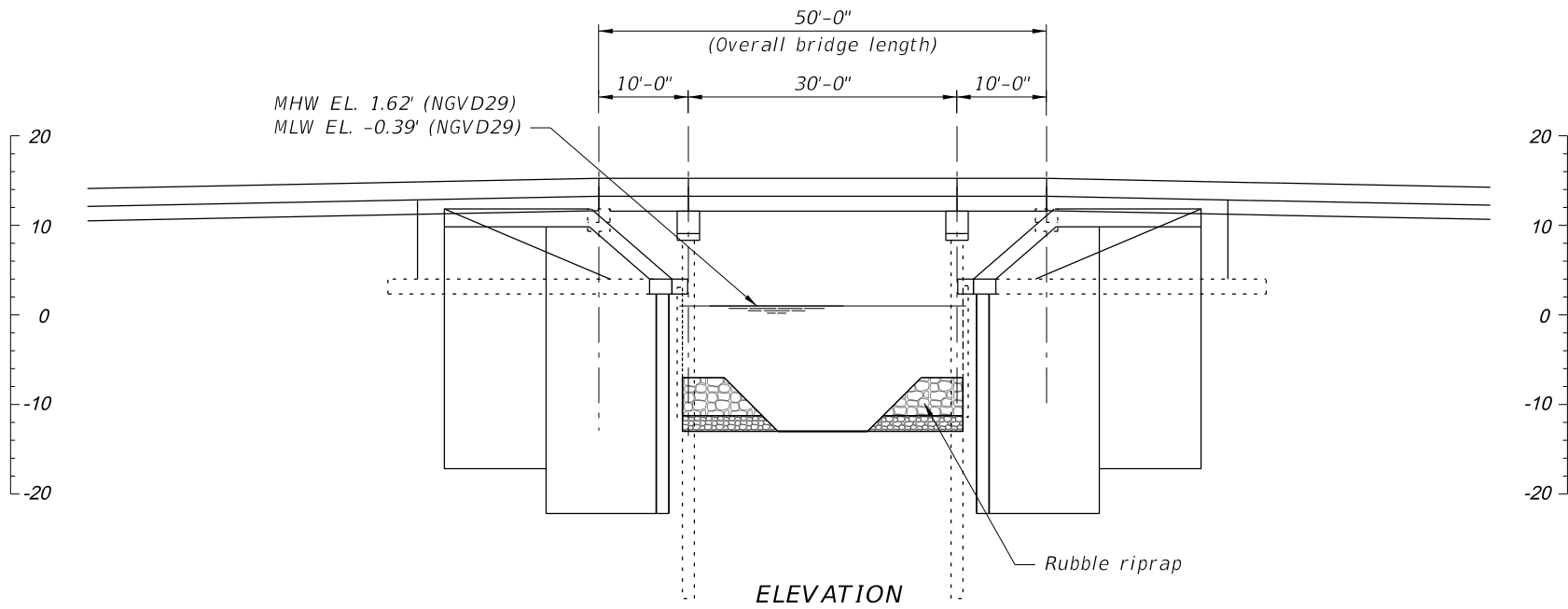
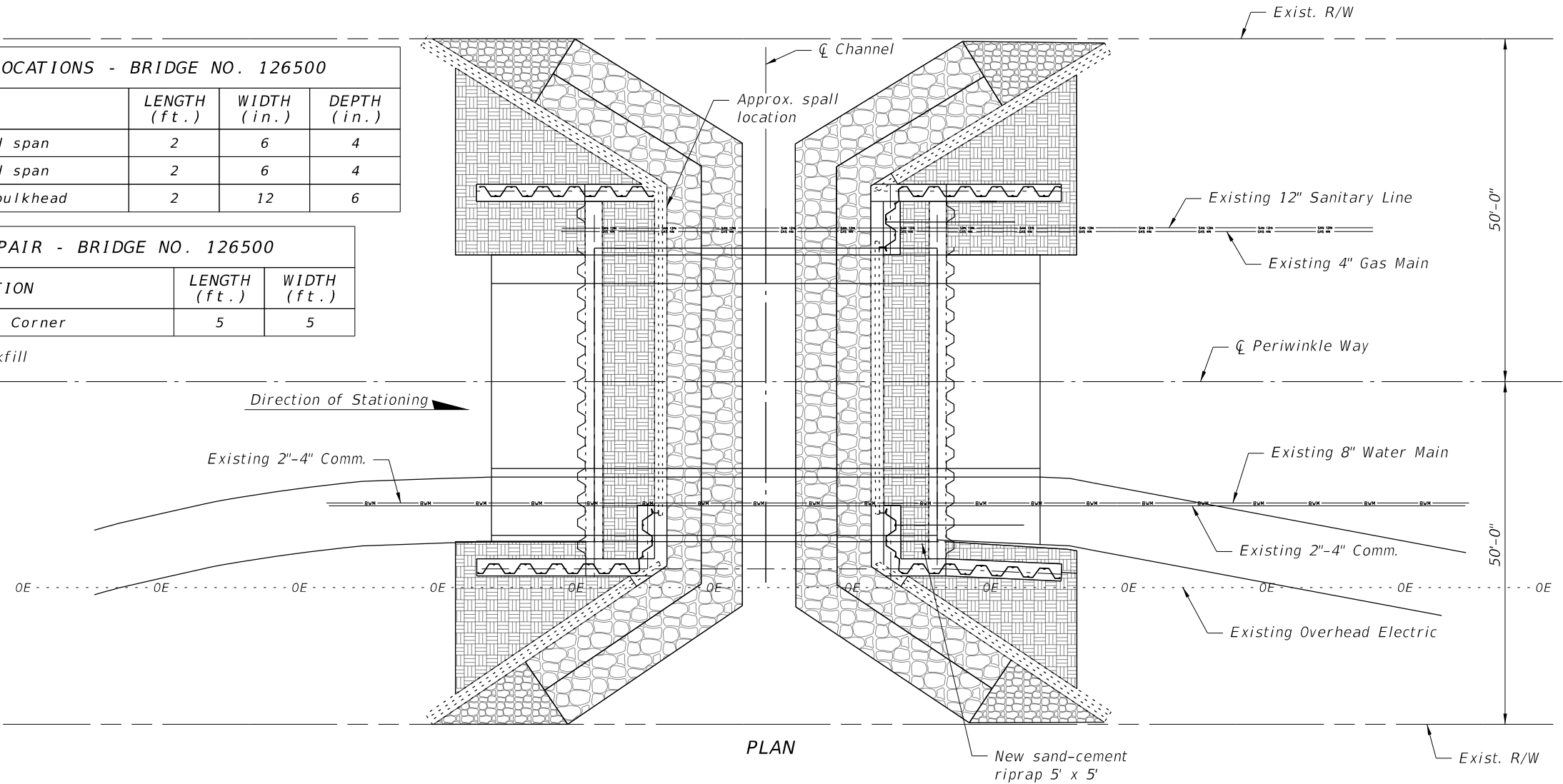
SHEET  
NO.

B4-1

SPALL REPAIR LOCATIONS - BRIDGE NO. 126500			
LOCATION	LENGTH (ft.)	WIDTH (in.)	DEPTH (in.)
Slab Unit 2-7 Mid span	2	6	4
Slab Unit 2-8 Mid span	2	6	4
North West corner bulkhead	2	12	6

RIPRAP REPAIR - BRIDGE NO. 126500		
LOCATION	LENGTH (ft.)	WIDTH (ft.)
Southeast Corner	5	5

NOTE: Use 1 cu ft backfill



BRIDGE NO. 126500  
PERIWINKLE WAY

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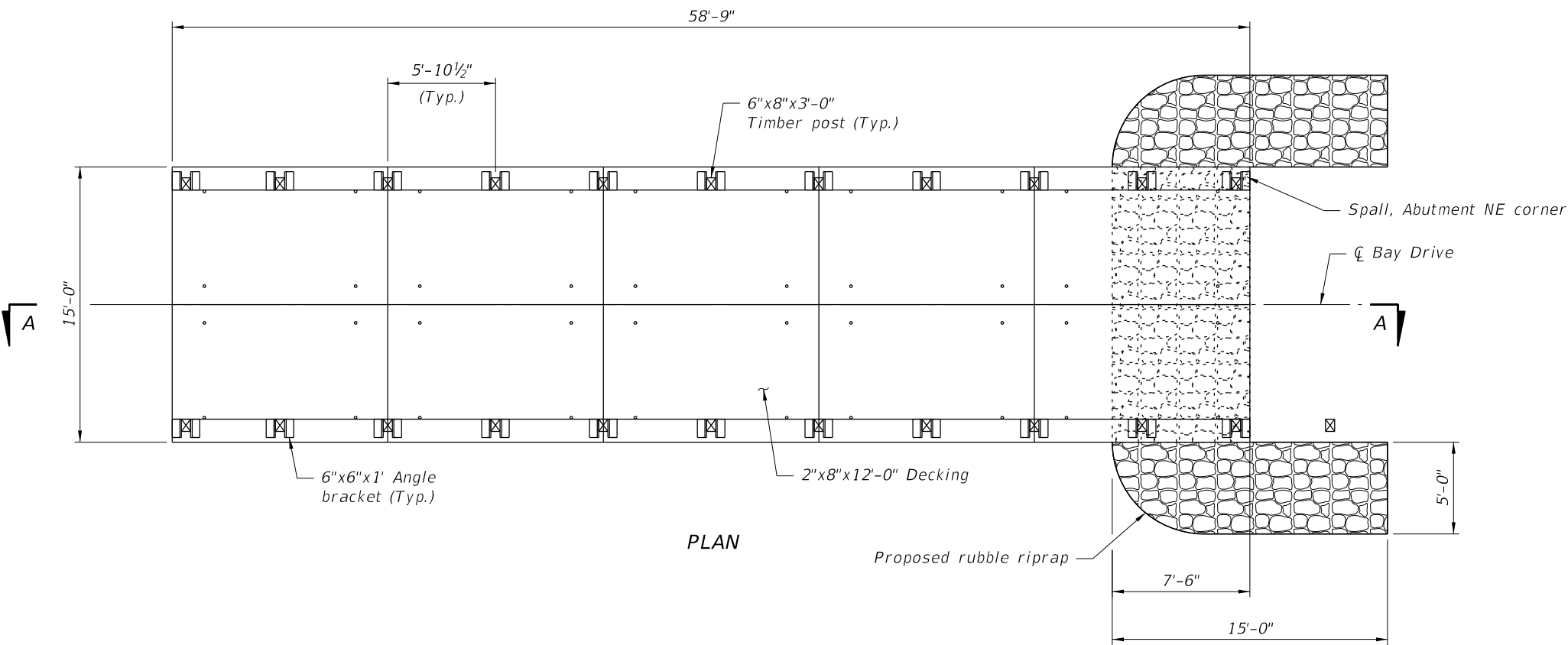
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**SANIBEL ISLAND STRUCTURES REPAIR**

SHEET  
NO.  
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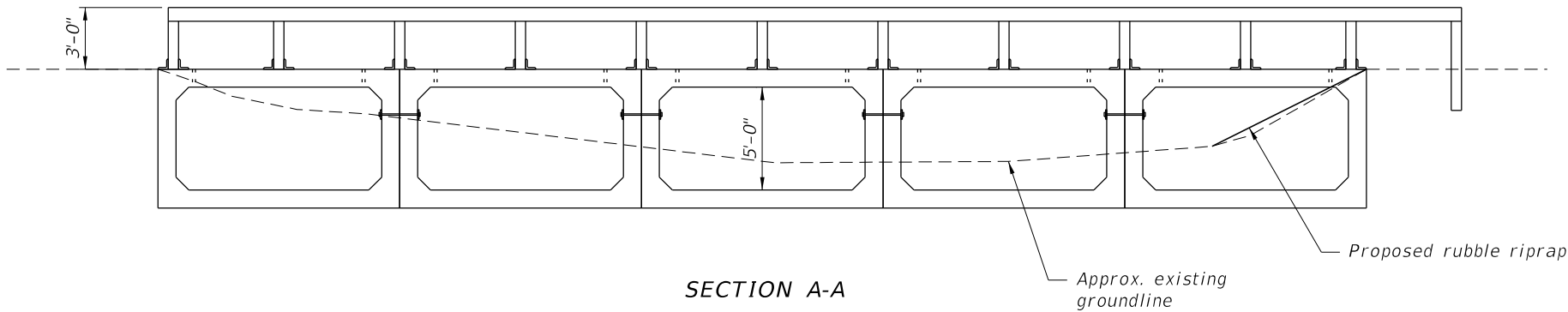
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PINE ISLAND  
SOUND



PLAN



SECTION A-A

SLOPE PROTECTION - BRIDGE NO. 126502

LOCATION	LENGTH (ft.)	WIDTH (ft.)
North East Corner	15	5
South East Corner	15	5

SPALL REPAIR LOCATIONS - BRIDGE NO. 126502

LOCATION	LENGTH (ft.)	WIDTH (in.)	DEPTH (in.)
Span 3, Bottom slab North End	2	9	4
Pier 4	2	9	4
Span 2, Top slab	0.5	6	4
Span 4, Bottom slab mid	1.5	18	4
Abutment 6 NE Corner	2	24	6

BRIDGE NO. 126502  
BAY DRIVE OVER TIDAL CANAL

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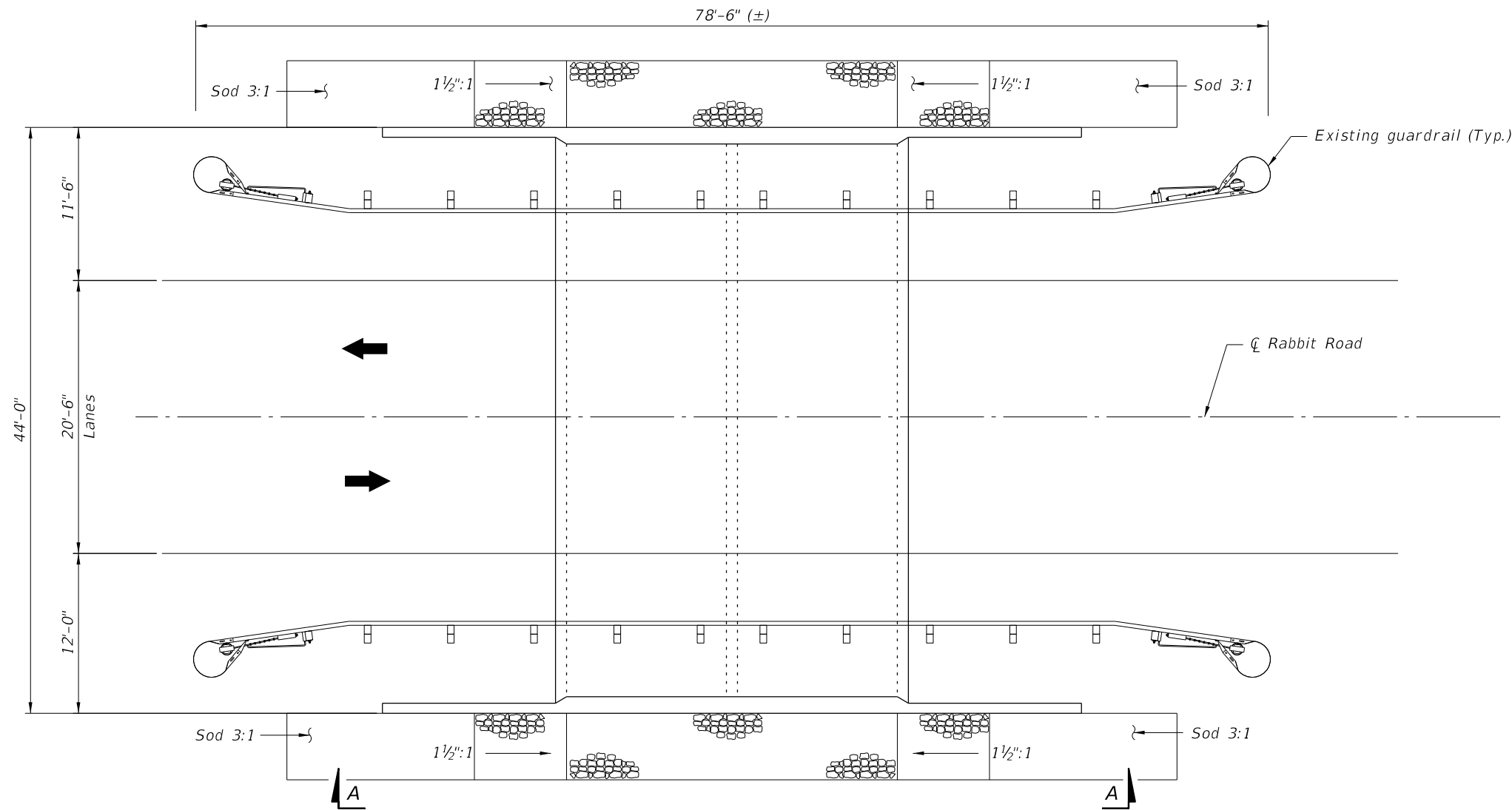
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PLAN AND ELEVATION  
**SANIBEL ISLAND STRUCTURES REPAIR**

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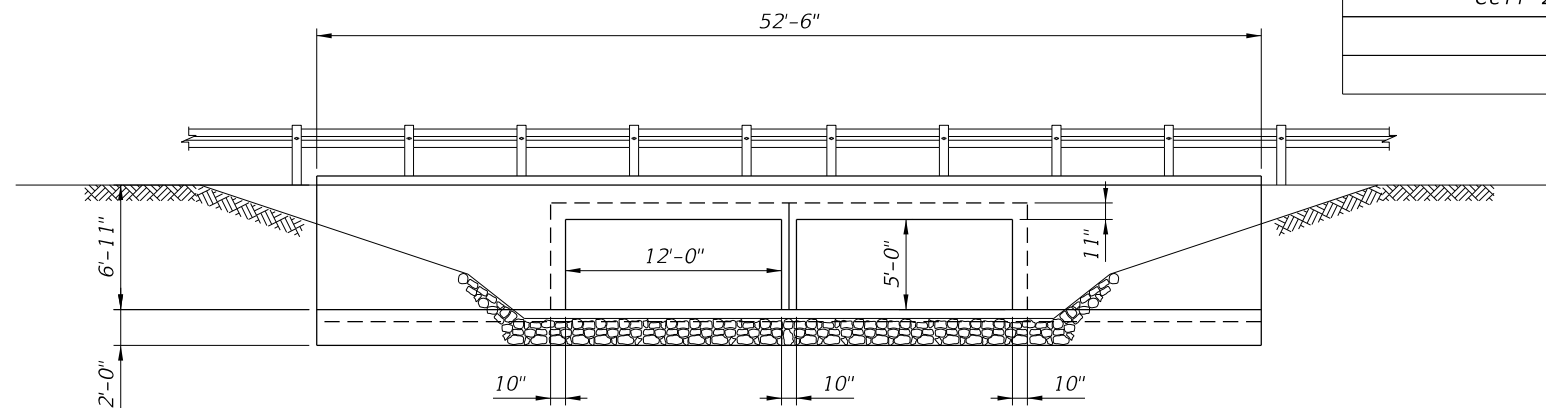
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PLAN

SPALL REPAIR LOCATIONS - BRIDGE NO. 126504			
LOCATION	LENGTH (in.)	WIDTH (in.)	DEPTH (in.)
Cell 2, Wall 3 West	9	6	4



VIEW A-A

BRIDGE NO. 126504  
RABBIT ROAD OVER SANIBEL RIVER

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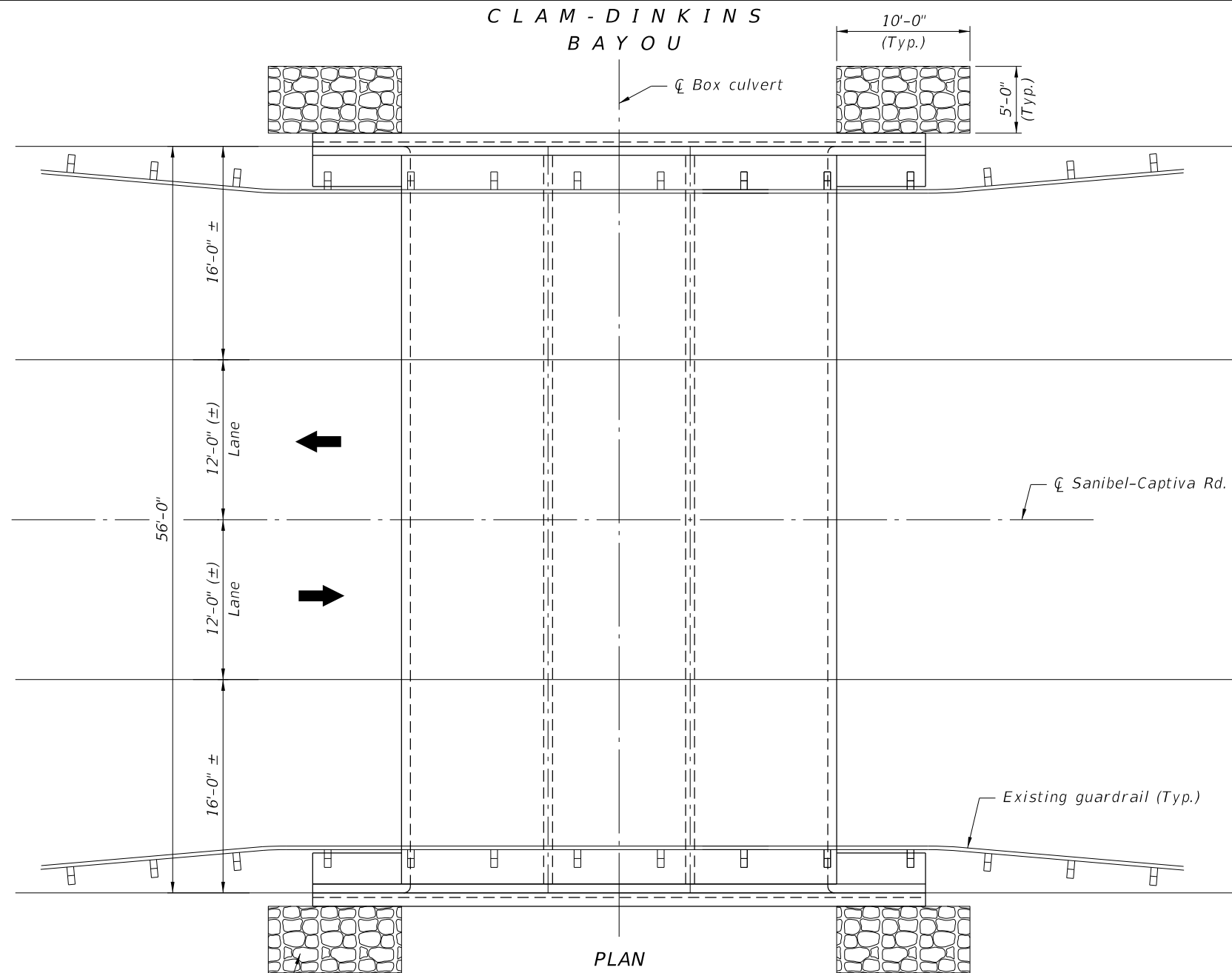
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PLAN AND ELEVATION  
**SANIBEL ISLAND STRUCTURES REPAIR**

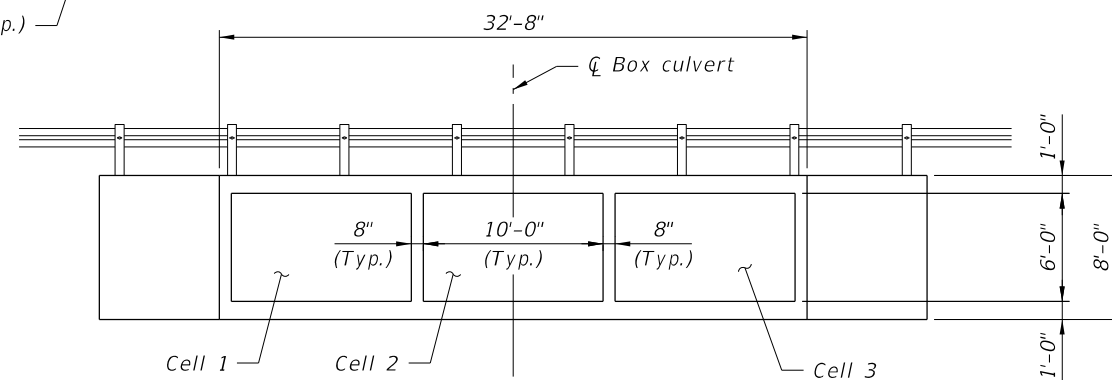
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PLAN



ELEVATION

SLOPE PROTECTION - BRIDGE NO. 126506		
LOCATION	LENGTH (ft.)	WIDTH (ft.)
Northeast Corner	10	5
Northwest Corner	10	5
Southeast Corner	10	5
Southwest Corner	10	5

SPALL REPAIR LOCATIONS - BRIDGE NO. 126506			
LOCATION	LENGTH (ft.)	WIDTH (ft.)	DEPTH (in.)
South Headwall	3	2	6
Northeast Wingwall	4	1.5	5
Cell 1, Wall 2 north	1.5	0.5	4
Cell Corrosion bleed out	0.5	0.5	4

BRIDGE NO. 126506  
SANIBEL-CAPTIVA RD OVER CLAM DINKINS BAYOU

NO.	DATE	BY	REVISIONS - DESCRIPTION

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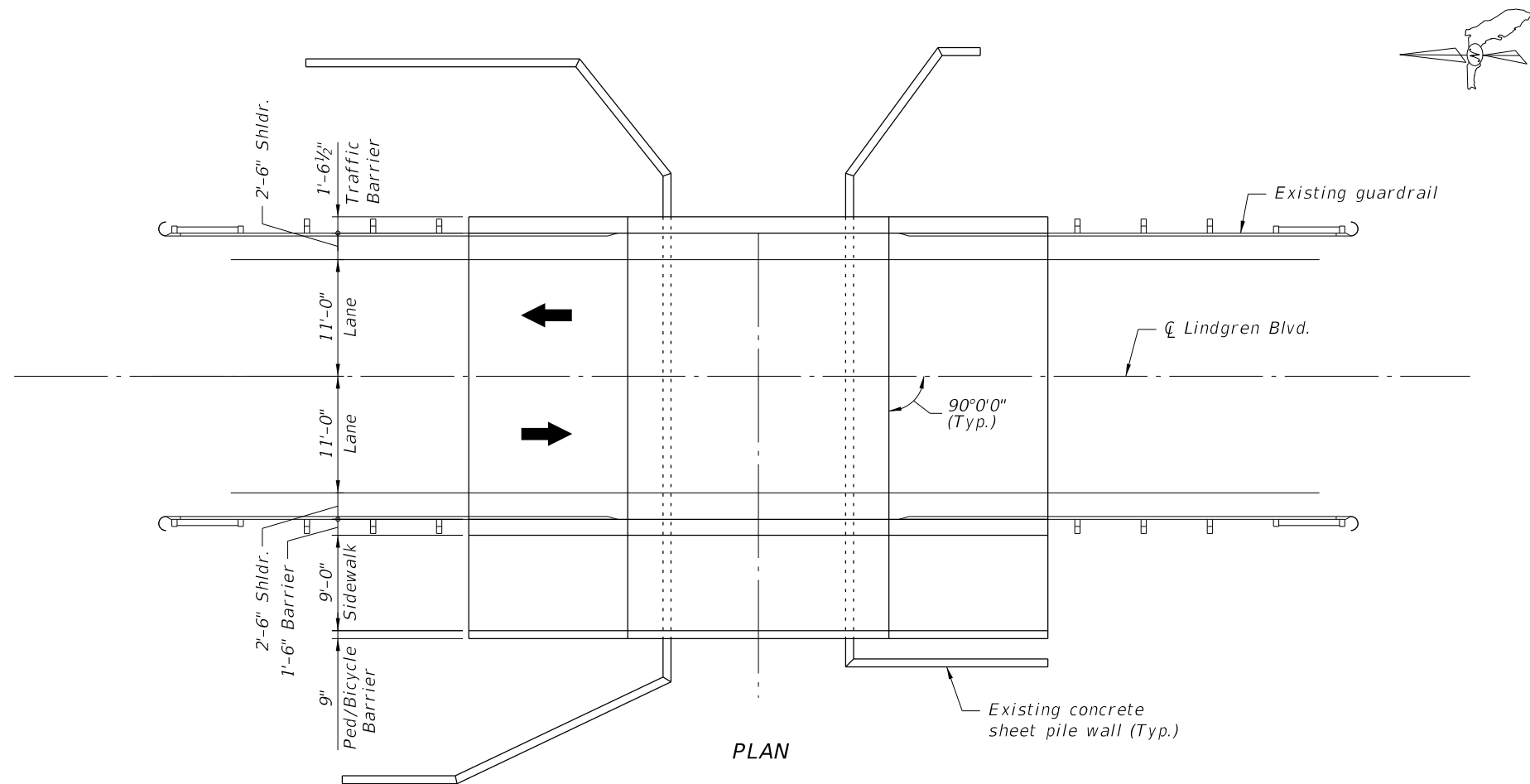
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PLAN AND ELEVATION
SANIBEL ISLAND STRUCTURES REPAIR

SHEET  
NO.

B8-1

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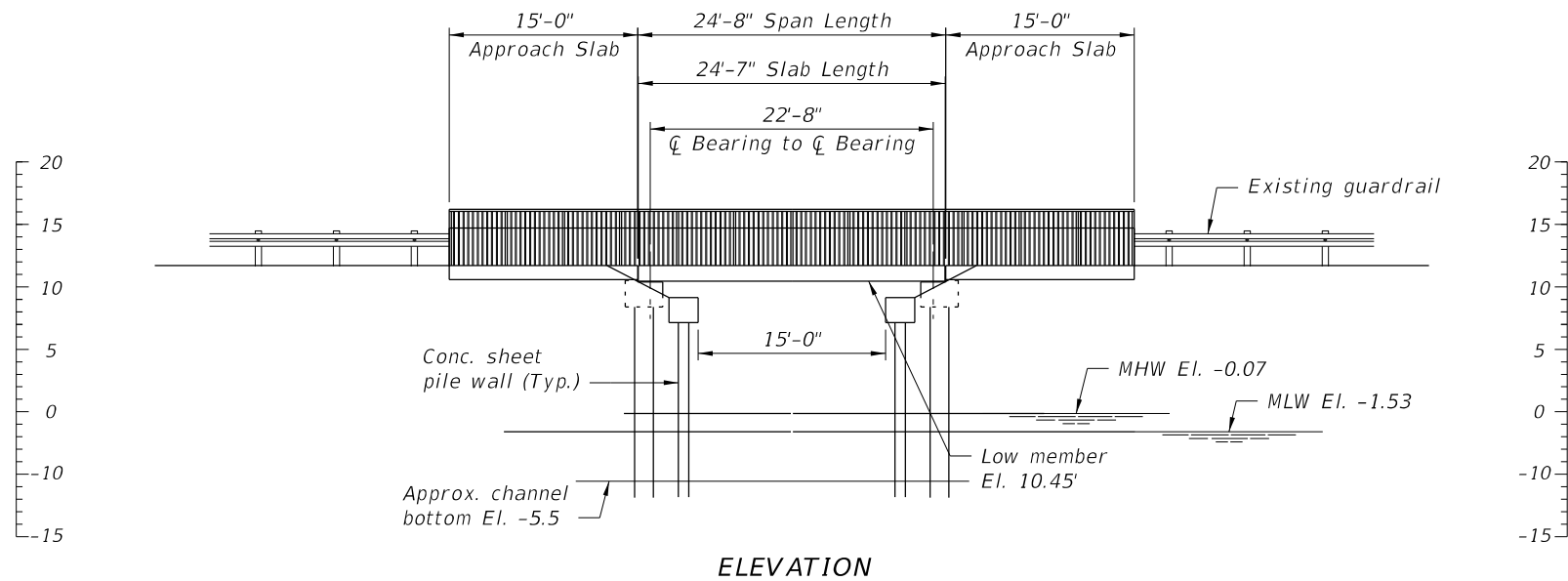


SPALL REPAIR LOCATIONS - BRIDGE NO. 126507			
LOCATION	LENGTH (in.)	WIDTH (in.)	DEPTH (in.)
Lane 1 Abutment 1	18	3	4
Wall 1, 3 full panels	2.5	18	4
Wall 1, 6 full panels	12	6	4

POURED JOINT - BRIDGE NO. 126507	
LOCATION	LENGTH (ft.)
Lane 1 Abutment 1	1
Abutment Cap West	0.5
Abutment to bulkhead	20

GUARDRAIL - BRIDGE NO. 126507	
LOCATION	LENGTH (ft.)
Southeast End	14

CEMENT GROUT - BRIDGE NO. 126507	
LOCATION	LENGTH (CF)
Bridge south approach roadway	1



BRIDGE NO. 126507  
LINDGREN BLVD OVER THE SANIBEL RIVER

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**B9-1**